



DEPARTMENT OF THE ARMY
U.S. ARMY CORPS OF ENGINEERS, NEW ENGLAND DISTRICT
NEW ENGLAND DISTRICT OFFICE
696 VIRGINIA ROAD
CONCORD MASSACHUSETTS 01742-2751

June 25, 2024

Regulatory Division
Connecticut & Rhode Island Section

Jeremy Hall
Connecticut Department of Energy and Environmental Protection
79 Elm Street
Hartford, CT 06106
[sent via email: Jeremy.hall@ct.gov]

Dear Mr. Hall:

The U.S. Army Corps of Engineers, New England District Regulatory Division (USACE) has reviewed your March 27, 2024, application to remove the existing, deteriorated Eagle Landing Fishing Pier and replace it with a similar structure. Thirty-five (35) 12-inch piles will be removed using a vibratory hammer. If complete extraction of any of the existing piles is not achievable or successful, the pile will be cut two feet below the mudline. The existing bulkhead consisting of four stacked 3-inch by 12-inch timbers will be demolished and removed. Thirty-six (36) 12-inch diameter timber piles will be installed via a combination of vibratory and impact hammers to support a new T-shaped pier. Final dimensions of the pier will be 66 feet by 9 feet and 98 feet by 9 feet. A temporary cofferdam made of sandbags or steel sheeting will be constructed around the bulkhead area to create an approximately 78 square foot dry space to install a new timber bulkhead consisting of four stacked 3-inch by 12-inch timbers. Up to 0.81-cubic yard (CY) of existing fill material from behind the existing bulkhead will be relocated to fill behind the new bulkhead, impacting up to 9.62 square feet below the High Tide Line (HTL) of the Connecticut River.

The project is located at Latitude 41.448010° N., Longitude 72.464410° W.; 14 Little Meadow Road, Haddam, Middlesex County, Connecticut. Work will be performed in accordance with the enclosed plans, sheets 1-10, dated July 14, 2023, and October 13, 2023.

Based on the information you have provided, we verify that the activity is authorized under General Permit 2, Repair or Maintenance of Existing or Currently Serviceable, Authorized or Grandfathered Structures and Fills, Removal of Structures, of the December 15, 2021, federal permit known as the Connecticut Regional General Permits (GPs). If the extent of the project area and/or nature of the authorized impacts to waters are modified, a revised application must be submitted to this office for written approval before work is initiated. A copy of these permits can be found at:
<https://www.nae.usace.army.mil/Missions/Regulatory/State-General-Permits/>.

Any deviation from the terms and conditions of the permit, or your submitted plans, may subject you, the permittee, to the enforcement provisions of our regulations. Therefore, in the event changes to this project are required, it is recommended you coordinate with this office prior to proceeding with the work. This office must approve any changes before you undertake them. You must perform this work in compliance with the terms and conditions of the GP listed above, and also in compliance with the following special conditions:

Project Specific Special Conditions:

1. The permittee shall complete and return the enclosed Work-Start Notification Form to this office at least two weeks prior to the anticipated construction start date.
2. The permittee shall complete and return the enclosed Completion Certification Form to this office at least one month following the completion of the authorized work.
3. You must maintain the activity herein in good condition and in conformance with the terms and conditions of this authorization. You are not relieved of this requirement if you abandon the permitted activity, although you may make a good faith transfer to a third party in compliance with a General Condition of this GPs. Should you wish to cease to maintain the authorized activities, or should you desire to abandon it without a good faith transfer, you must obtain a modification of this authorization from this office, which may require restoration of the area.
4. For impact pile installation: Pile driving will commence with an initial set of three strikes by the hammer at 40% energy, followed by a one-minute waiting period, then two subsequent 3-strike sets at 40% energy, with one-minute waiting periods, before initiating continuous impact driving.

For vibratory pile installation: Pile driving will be initiated for 15 seconds at reduced energy followed by a one-minute waiting period. This sequence of 15 seconds of reduced energy driving, one-minute waiting period will be repeated two additional times, followed immediately by pile driving at full rate and energy.

These procedures will be implemented prior to pile installation commencing at full strength and must be implemented following cessation of pile driving for a period of 30 minutes or longer.

This verification is valid until December 15, 2026. You must commence or be under contract to commence the work authorized herein by this date and complete the work by December 15, 2027. If work is not completed by this date, you must contact this office to determine the need for further authorization before beginning or continuing the activity.

It is recommended that you contact this office before this authorization expires to discuss if permit reissuance is a possibility.

This GP verification and any associated authorizations does not preclude the necessity to obtain any other Federal, State, or local permits, licenses, and/or certifications, which may be required.

If you have any questions related to this verification or have issues accessing documents referenced in this letter, please contact Roberta Budnik at (978) 318-8766, or by email at roberta.k.budnik@usace.army.mil. This agency continually strives to improve our customer service. In order to better serve you, please complete the Customer Service Survey located at: <https://regulatory.ops.usace.army.mil/customer-service-survey/>.

Sincerely,

Kevin R Kotelly

Kevin Kotelly, PE
Chief, CT/RI Section

Enclosures

Cc (w/enclosures):

Genevieve Nuttall, AECOM

Sabrina Pereira, NMFS

Nathan Margason, EPA

genevieve.nuttall@aecom.com

Sabrina.pereira@noaa.gov

Margason.nathan@epa.gov

Work-Start Notification Form

File Number: NAE-2024-00839

State: Connecticut County: Middlesex

**Permittee: Connecticut Department of Energy and Environmental Protection,
Jeremy Hall**

Date Verification Issued: 6/12/2024

Project Manager: Roberta Budnik

At least two weeks prior to commencing the activity authorized by this permit, sign this certification and return it to the following address:

**US ARMY CORPS OF ENGINEERS
New England District
Attn: Roberta Budnik
696 Virginia Road
Concord, MA 01742
or
roberta.k.budnik@usace.army.mil**

Please note that your permitted activity is subject to a compliance inspection by a U. S. Army Corps of Engineers (USACE) representative. Failure to comply with any terms or conditions of this authorization may result in the USACE suspending, modifying or revoking the authorization and/or issuing a Class I administrative penalty, or initiating other appropriate legal action.

The people (e.g. contractor) listed below will do the work, and they understand the permit's conditions and limitations.

Contractor Name/Contractor Firm: _____

Business Address: _____

Contractor Phone and Email: _____

Proposed Construction Dates: Start: _____ **Finish:** _____

Signature of Permittee

Date

Compliance Certification Form

File Number: NAE-2024-00839

State: Connecticut County: Middlesex

Permittee: Connecticut Department of Energy and Environmental Protection, Tom Tyler

Date Verification Issued: 6/12/2024

Project Manager: Roberta Budnik

Within one month of completion of the activity authorized by this permit and any mitigation required by the permit (you must submit this form after mitigation is complete, but not the mitigation monitoring, which requires separate submittals), sign this certification and return it to the following address:

**US ARMY CORPS OF ENGINEERS
New England District
Attn: Roberta Budnik
696 Virginia Road
Concord, MA 01742
or
roberta.k.budnik@usace.army.mil**

Please note that your permitted activity is subject to a compliance inspection by a U. S. Army Corps of Engineers (USACE) representative. Failure to comply with any terms or conditions of this authorization may result in the USACE suspending, modifying, or revoking the authorization and/or issuing a Class I administrative penalty, or initiating other appropriate legal action.

I hereby certify that the work, and mitigation (if applicable), authorized by the above referenced permit has been completed in accordance with the terms and conditions of the said permit including any general or specific conditions.

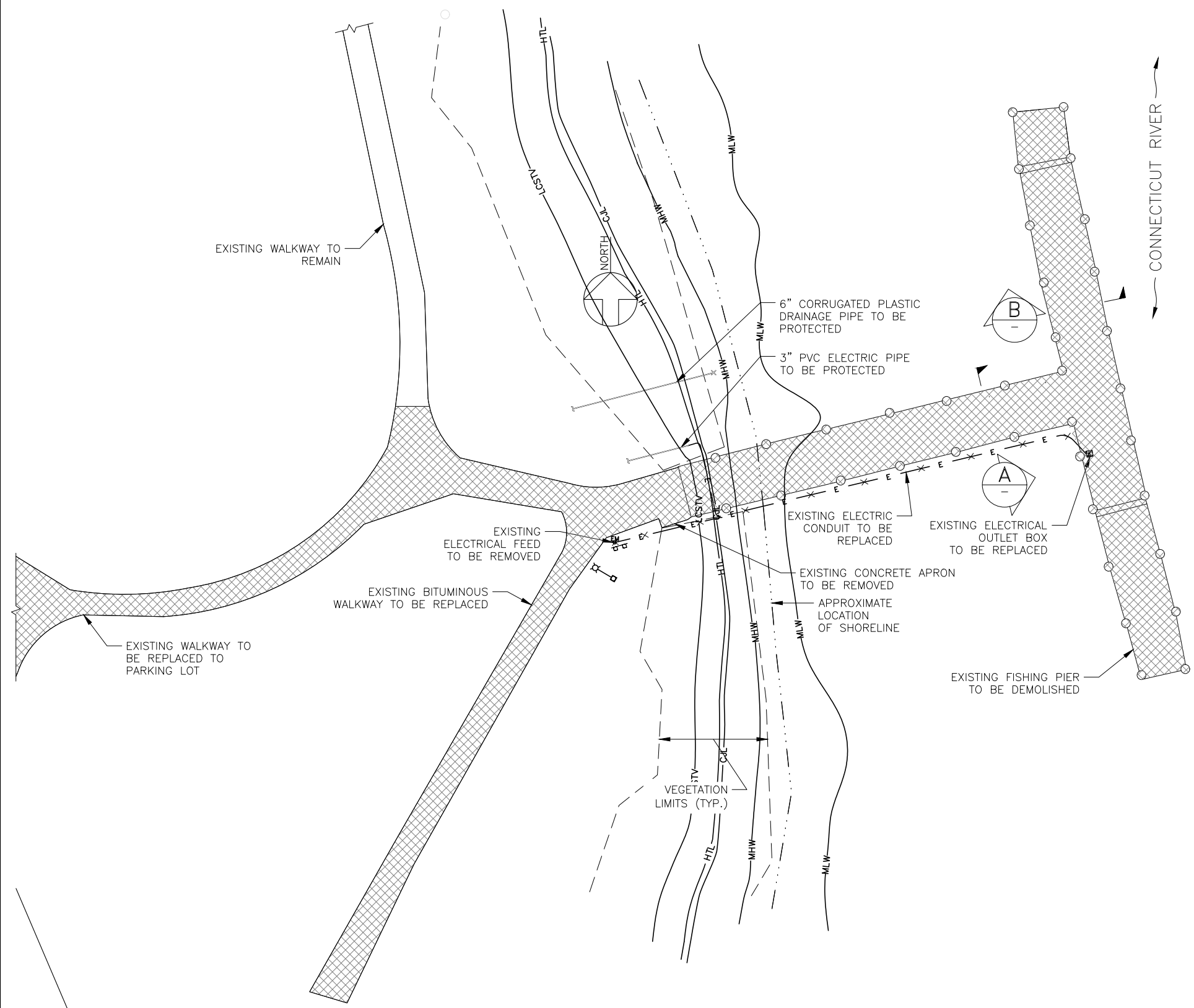
Date Authorized Work Started: _____ **Completed:** _____

Describe any deviations from permit (attach drawing(s) depicting the deviations):

***Note: The description of any deviations on this form does not constitute approval by the USACE.**

Signature of Permittee

Date



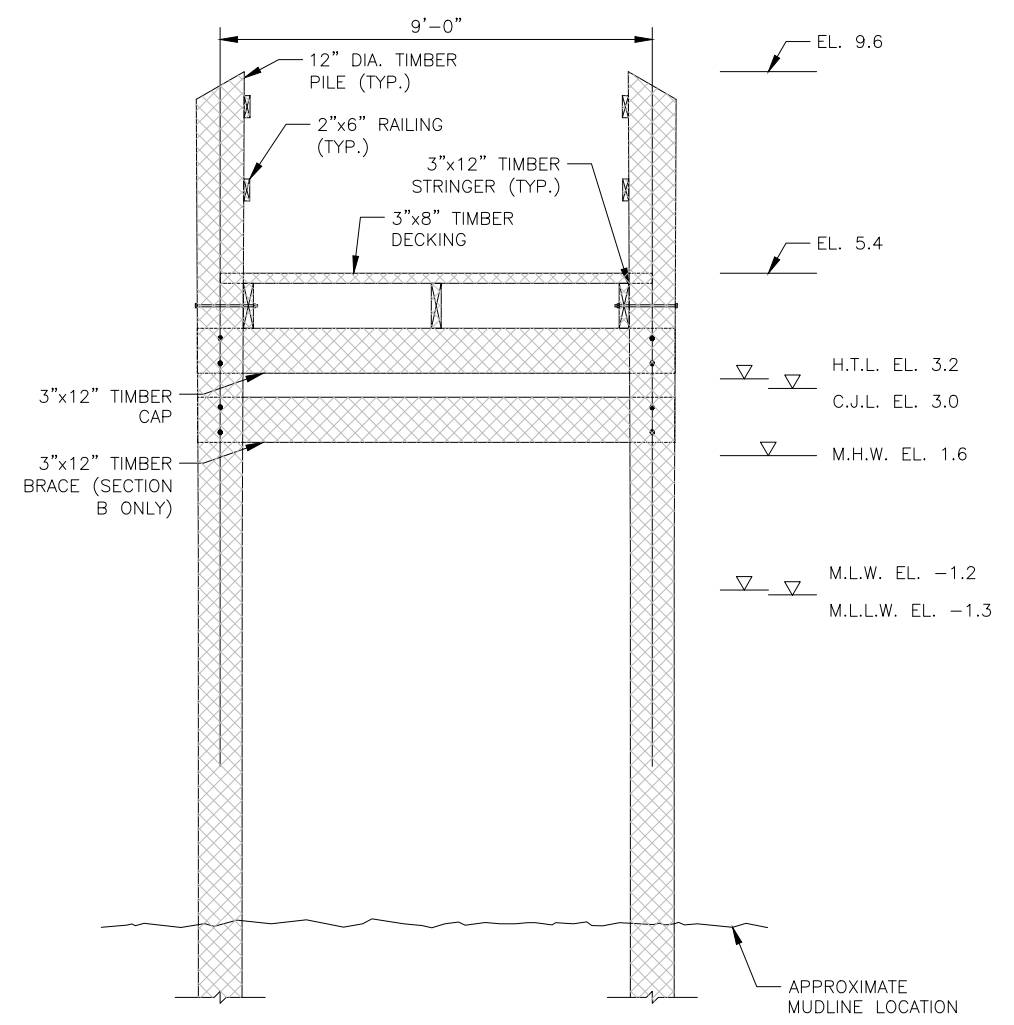
NOTES:

1. REFER TO DWG. NO. FL-01 FOR GENERAL FISHING PIER NOTES.
2. INFORMATION IN THIS PLAN SET INCLUDING BUT NOT LIMITED TO TOP OF BANK LOCATION, EXISTING PIER DIMENSIONS, AND LIMITS OF PROPOSED PIER REPLACEMENT SHOWN ARE APPROXIMATE AND SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO PERFORMING ANY WORK.
3. ALL ELEVATIONS AND DIMENSIONS TO BE FIELD VERIFIED PRIOR TO CONSTRUCTION.

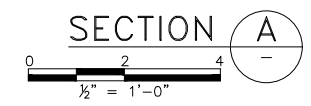
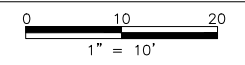
LEGEND:

EXISTING STRUCTURE TO BE DEMOLISHED

REGULATORY ELEVATIONS	NAVD88
MEAN LOWER LOW WATER (M.L.L.W.)	-1.3 FT
MEAN LOW WATER (M.L.W.)	-1.2 FT
MEAN HIGH WATER (M.H.W.)	1.6 FT
COASTAL JURISDICTION LINE (C.J.L.)	3.0 FT
HIGH TIDE LINE (H.T.L.)	3.2 FT



EXISTING FISHING PIER DEMOLITION PLAN



FILE NAME: 3496-EL-01_EAGLE LANDING DEMOLITION.DWG
DATE PLOTTED: 7/14/2023 10:58:15 AM
STANDARD PLOT TABLE YES

No.	Revisions	Date	By
0	ADDENDUM NO. 5 - NEW SHEET ADDED	10/13/23	HH

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**Office of Chief Engineer
STRUCTURES**
 National Railroad Passenger Corporation
 30th Street Station, Philadelphia, Pennsylvania 19104

Approved	Date

HARDESTY & HANOVER, LLC
 ENGINEERING
 1501 Broadway New York, NY 10036

OLD SAYBROOK CONNECTICUT
**REPLACEMENT OF MB 106.89
OVER CONNECTICUT RIVER**
EAGLE LANDING FISHING PIER DEMOLITION
 Designed MY Drawn PD Checked MY Date 7/14/2023

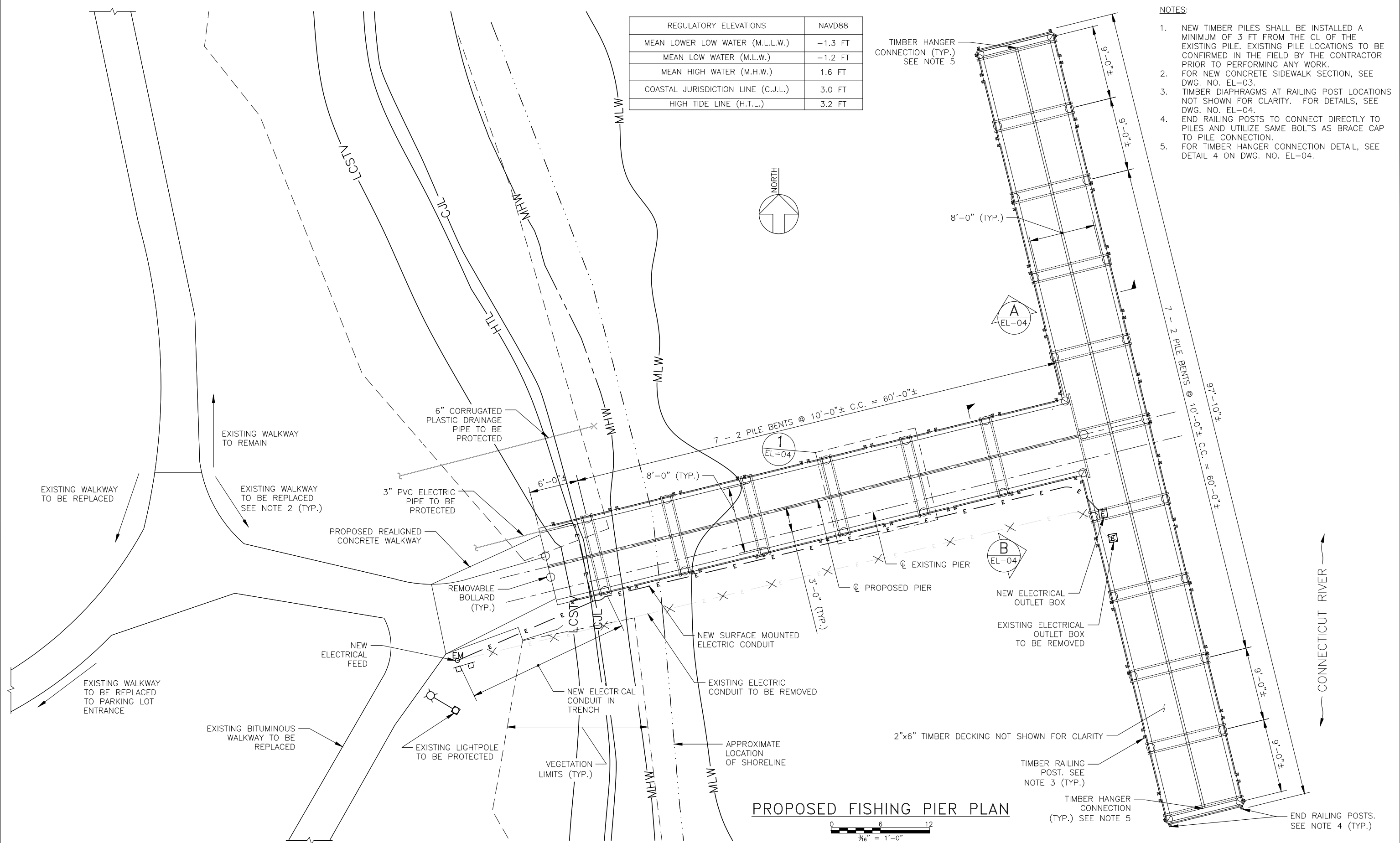
Project Code: CT106.89
Sheet No. OF 000
Dwg. No. EL-01

REGULATORY ELEVATIONS	NAVD88
MEAN LOWER LOW WATER (M.L.L.W.)	-1.3 FT
MEAN LOW WATER (M.L.W.)	-1.2 FT
MEAN HIGH WATER (M.H.W.)	1.6 FT
COASTAL JURISDICTION LINE (C.J.L.)	3.0 FT
HIGH TIDE LINE (H.T.L.)	3.2 FT

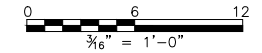
TIMBER HANGER CONNECTION (TYP.) SEE NOTE 5

NOTES:

1. NEW TIMBER PILES SHALL BE INSTALLED A MINIMUM OF 3 FT FROM THE CL OF THE EXISTING PILE. EXISTING PILE LOCATIONS TO BE CONFIRMED IN THE FIELD BY THE CONTRACTOR PRIOR TO PERFORMING ANY WORK.
2. FOR NEW CONCRETE SIDEWALK SECTION, SEE DWG. NO. EL-03.
3. TIMBER DIAPHRAGMS AT RAILING POST LOCATIONS NOT SHOWN FOR CLARITY. FOR DETAILS, SEE DWG. NO. EL-04.
4. END RAILING POSTS TO CONNECT DIRECTLY TO PILES AND UTILIZE SAME BOLTS AS BRACE CAP TO PILE CONNECTION.
5. FOR TIMBER HANGER CONNECTION DETAIL, SEE DETAIL 4 ON DWG. NO. EL-04.



PROPOSED FISHING PIER PLAN



FILE NAME: 3496_EL-02_EAGLE LANDING PROPOSED.DWG
DATE PLOTTED: 7/14/2023 10:58:58 AM
STANDARD PEN TABLES

No.	Revisions	Date	By
0	ADDENDUM NO. 5 - NEW SHEET ADDED	10/13/23	HH

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


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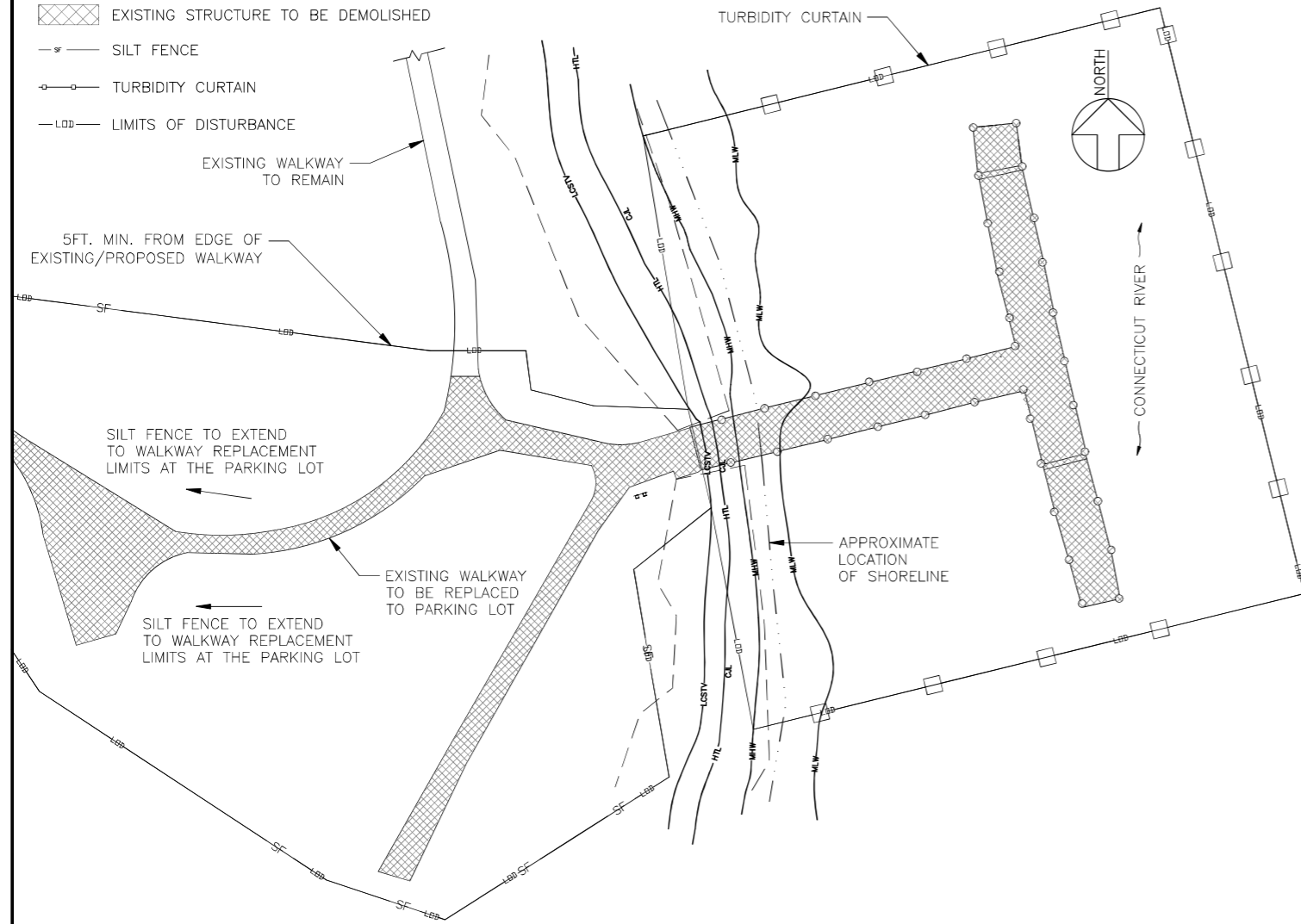
Approved	Date

H&H HARDESTY & HANOVER, LLC
 ENGINEERING
 1501 Broadway
 New York, NY 10036

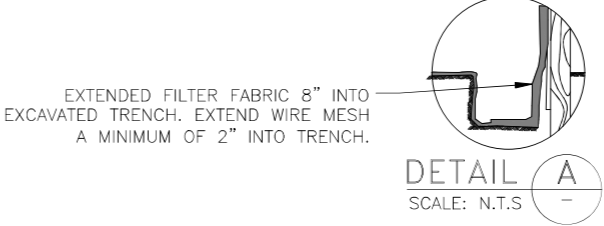
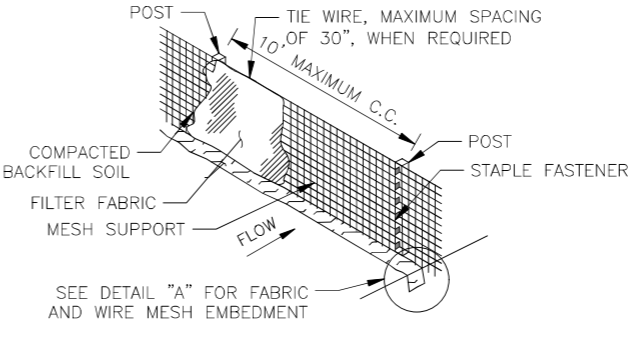
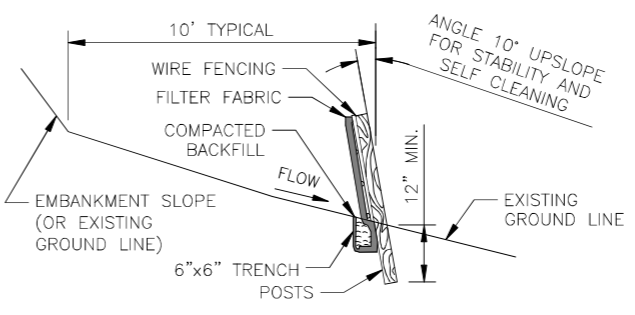
OLD SAYBROOK CONNECTICUT Project Code: CT106.89
REPLACEMENT OF MB 106.89 OVER CONNECTICUT RIVER
EAGLE LANDING FISHING PIER PROPOSED PLAN
 Sheet No. EL-02
 Dwg. No. EL-02
 Designed MY Drawn PD Checked MY Date 7/14/2023

LEGEND:

-  EXISTING STRUCTURE TO BE DEMOLISHED
-  SILT FENCE
-  TURBIDITY CURTAIN
-  LIMITS OF DISTURBANCE



EROSION CONTROL PLAN
 0 20 40
 1" = 20'



TEMPORARY SILT FENCE DETAILS
 SCALE: N.T.S.

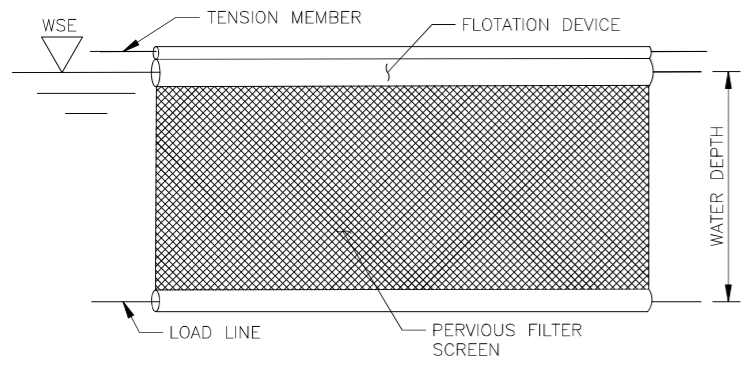
EROSION AND SEDIMENTATION CONTROL PLAN NOTES:

1. EROSION AND SEDIMENTATION CONTROLS MUST BE CONSTRUCTED, STABILIZED, AND FUNCTIONAL BEFORE GENERAL SITE DISTURBANCE WITHIN THE TRIBUTARY AREAS OF THOSE CONTROLS.
2. AFTER FINAL SITE STABILIZATION HAS BEEN ACHIEVED, TEMPORARY EROSION AND SEDIMENTATION CONTROLS MUST BE REMOVED. AREAS DISTURBED DURING REMOVAL OF THE CONTROLS MUST BE STABILIZED.
3. EROSION AND SEDIMENTATION CONTROLS MUST BE PROTECTED FROM UNAUTHORIZED ACTS OF THIRD PARTIES IN ACCORDANCE WITH SPECIFICATION 1399.1.11.
4. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PROPER CONSTRUCTION, STABILIZATION, AND MAINTENANCE OF ALL EROSION AND SEDIMENTATION CONTROLS AND RELATED ITEMS INCLUDED WITHIN THIS PLAN.
5. THE CONTRACTOR MUST DEVELOP, AND HAVE APPROVED BY THE CONNECTICUT DEPARTMENT OF ENVIRONMENTAL PROTECTION, A SEPARATE EROSION AND SEDIMENTATION CONTROL PLAN FOR EACH SPOIL, BORROW, OR OTHER WORK AREA NOT DETAILED IN THE PERMITTED PLAN, WHETHER LOCATED WITHIN OR OUTSIDE OF THE CONSTRUCTION LIMITS.
6. SHOULD ANY MEASURES CONTAINED WITHIN THIS PLAN PROVE INCAPABLE OF ADEQUATELY REMOVING SEDIMENT FROM ON-SITE FLOWS PRIOR TO DISCHARGE OR OF STABILIZING THE SURFACES IMPACTED BY THE CONTRACTOR, THE CONTRACTOR SHALL ELIMINATE ALL SUCH PROBLEMS PRIOR TO CONTINUING WORK.
7. ON A WEEKLY BASIS AND AFTER EACH STORM EVENT, ALL EROSION AND SEDIMENTATION CONTROLS SHALL BE CHECKED UNTIL THE SITE IS STABILIZED. ALL PREVENTATIVE AND REMEDIAL MAINTENANCE WORK, INCLUDING CLEANOUT, REPAIR, REPLACEMENT, REGRADING, RESEEDING, REMULCHING, AND RENETTING MUST BE PERFORMED IMMEDIATELY.
8. EROSION AND SEDIMENTATION CONTROLS SHALL CONFORM TO THE REQUIREMENTS OF THE "2002 CONNECTICUT GUIDELINES FOR SOIL EROSION AND SEDIMENT CONTROL. CONNECTICUT COUNCIL ON SOIL AND WATER CONSERVATION (GUIDELINES)."
9. SYNTHETIC FILTER FABRIC SHALL BE A PERVIOUS SHEET OF PROPYLENE, NYLON, POLYESTER OR ETHYLENE FILAMENTS AND SHALL BE CERTIFIED BY THE MANUFACTURER OR SUPPLIER AS CONFORMING TO THE FOLLOWING REQUIREMENTS:

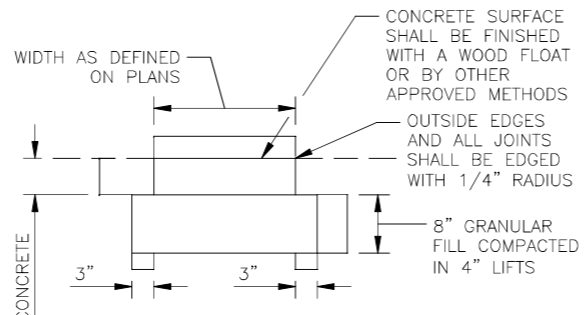
PHYSICAL PROPERTY	REQUIREMENTS
FILTERING EFFICIENCY	75% (MIN.)
TENSILE STRENGTH AT 20% (MAX.) ELONGATION	EXTRA STRENGTH - 50 lbs./ lin. in. (MIN.) STANDARD STRENGTH - 30 lbs./ lin. in. (MIN.)
FLOW RATE	0.3 gal./sq. ft. (MIN.)
10. PROVIDE FILTER FABRIC ALONG ALL INTERFACE AREAS WITH GROUND CONTACT.
11. SEE CONSTRUCTION GUIDELINES FOR SOIL EROSION AND SEDIMENT CONTROL (2002) FOR ADDITIONAL FILTER FENCE REQUIREMENTS.
12. PROVIDE APPROPRIATE TRANSITION BETWEEN CONSTRUCTION ENTRANCE AND ACCESS PATH, AS REQUIRED
13. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 013543 - ENVIRONMENTAL PROCEDURES AND SECTION 312000 - EARTH MOVING.

SIDEWALK NOTES:

10. DUMMY JOINTS TO BE 4' TYPICAL
11. EXPANSION JOINT TO BE 12" OR AS DIRECTED
12. CROSS SLOPE SHALL BE 1.5% TYPICAL, 2% MAXIMUM



TURBIDITY CONTROL CURTAIN DETAIL
 SCALE: N.T.S.
 TURBIDITY CURTAIN TO MEET CTDEEP CLASS IV STANDARDS



CONCRETE SIDEWALK
 SCALE: N.T.S.

FILE NAME: 348R_EL-03-EAGLE LANDING ESC PLAN.DWG
 PLOT SCALE: AS NOTED
 STANDARD PEN TABLE: YES

No.	Revisions	Date	By
0	ADDENDUM NO. 5 - NEW SHEET ADDED	10/13/23	HH

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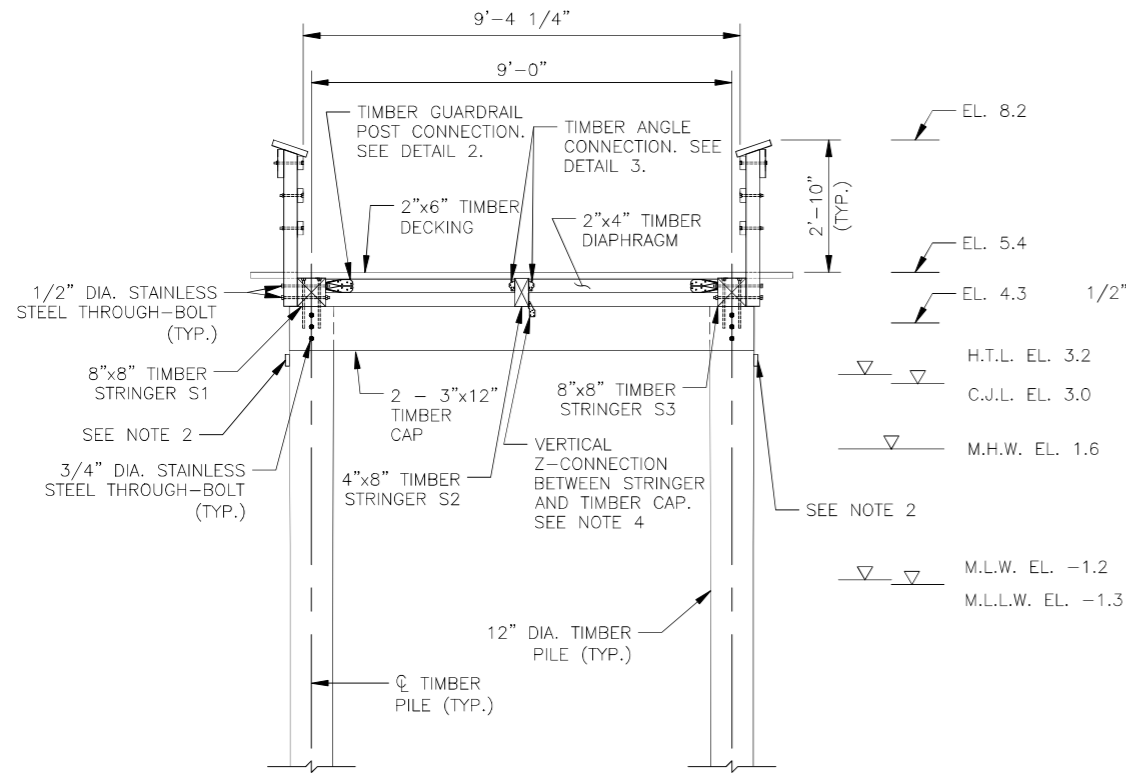
Office of Chief Engineer
STRUCTURES
 National Railroad Passenger Corporation
 30th Street Station, Philadelphia, Pennsylvania 19104

Approved	Date

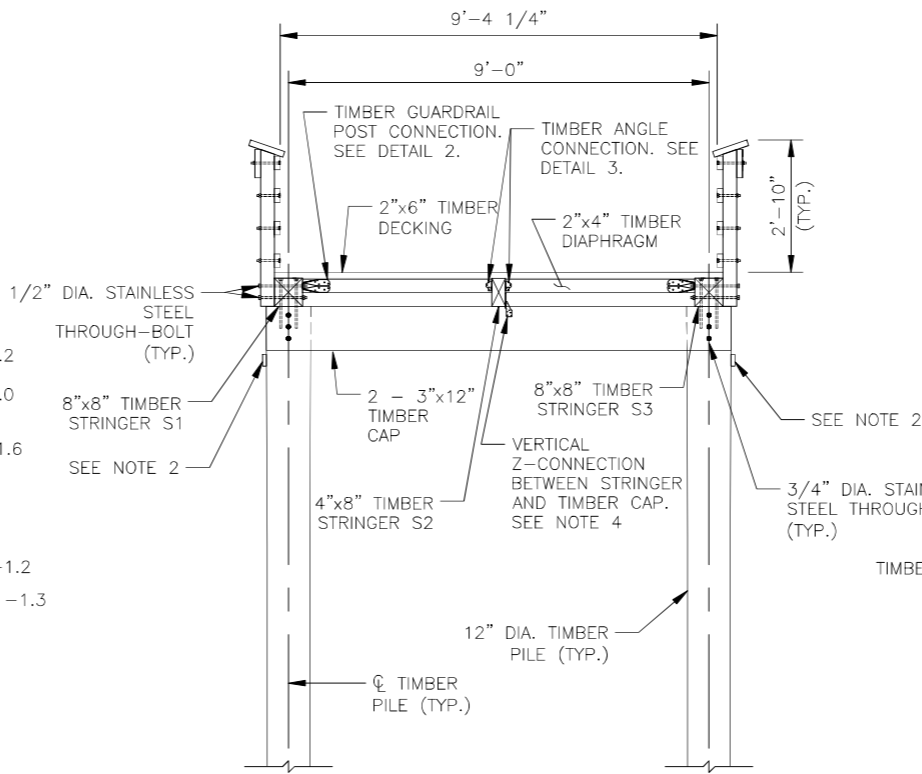


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 1501 Broadway
 New York, NY 10036

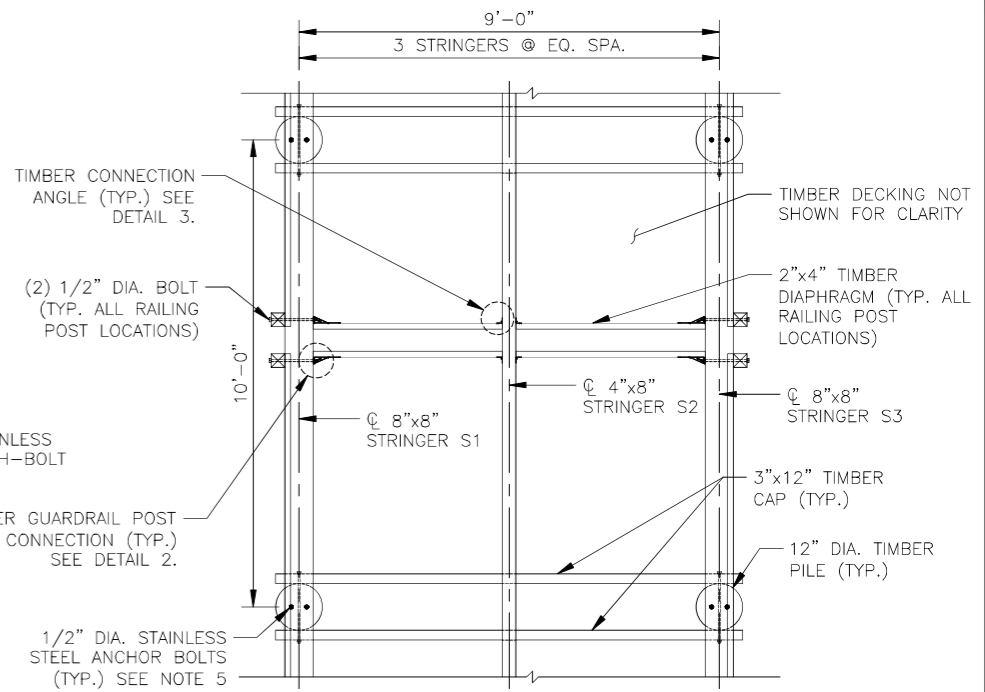
OLD SAYBROOK	CONNECTICUT	Project Code: CT106,89
REPLACEMENT OF MB 106.89 OVER CONNECTICUT RIVER		Sheet No. 1024.A5 OF 1026
EAGLE LANDING ESC PLAN		Dwg. No. EL-03
Designed MY	Drawn PS	Checked MY Date 10/13/2023



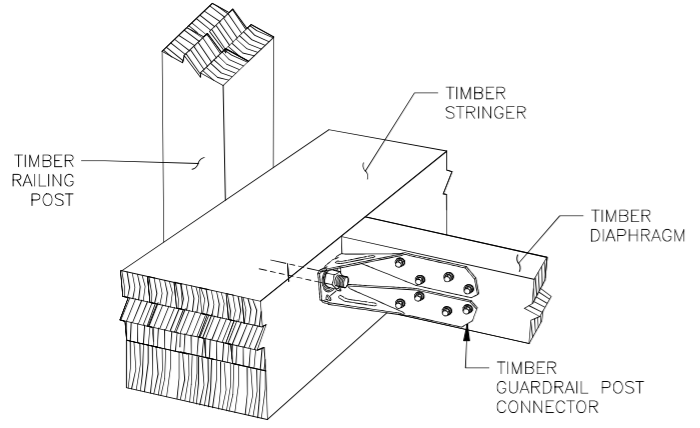
SECTION A
EL-02
0 2 4
1/2" = 1'-0"



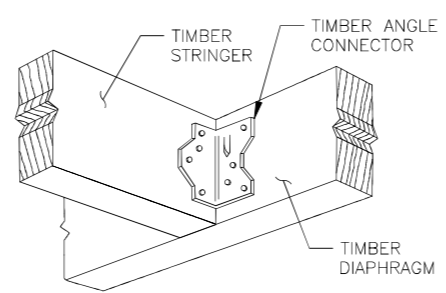
SECTION B
EL-02
0 2 4
1/2" = 1'-0"



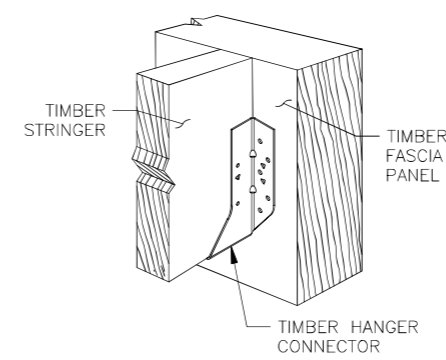
DETAIL 1
EL-02
0 2 4
1/2" = 1'-0"



DETAIL 2
N.T.S.



DETAIL 3
N.T.S.



DETAIL 4
N.T.S.

- NOTES:
- FOR HYDRAULIC DATA, SEE DWG. NO. EL-01.
 - (3) 3" DIAMETER WHITE REFLECTORS TO BE MOUNTED AT EACH PILE. CENTERLINE OF REFLECTOR TO BE AT EL. 3.5.
 - TIMBER FRAMING TO BE CUT AT AN ANGLE AS REQUIRED. TIMBER ANGLE CONNECTOR, HANGER CONNECTOR, AND Z-CONECTORS TO BE MODIFIED TO ACCOMMODATE THE CONNECTION AS NEEDED.
 - FOR Z-CONNECTION DETAILS, SEE DWG. NO. FL-08.
 - FOR ANCHOR BOLT DETAILS, SEE DWG. NO. EL-05.

FILE NAME: 3486_EL-04_EAGLE LANDING SECTIONS.DWG
PLOT SCALE: AS NOTED
STANDARD PEN TABLE: YES

No.	Revisions	Date	By
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National Railroad Passenger Corporation
30th Street Station, Philadelphia, Pennsylvania 19104

Approved	Date



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ENGINEERING
1501 Broadway
New York, NY 10036

OLD SAYBROOK CONNECTICUT Project Code: CT106,89

**REPLACEMENT OF MB 106.89
OVER CONNECTICUT RIVER**

EAGLE LANDING TYPICAL SECTIONS

Sheet No. 1025.A5 OF 1026
Dwg. No. **EL-04**

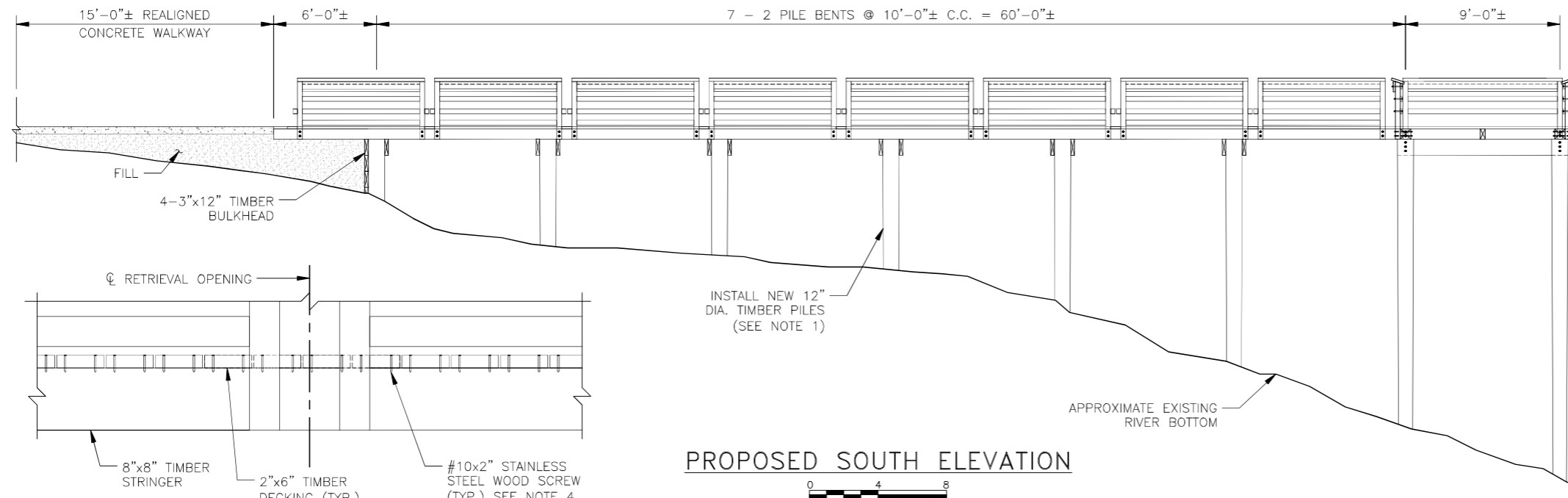
Designed MY Drawn PD Checked MY Date 10/13/2023

NOTES:

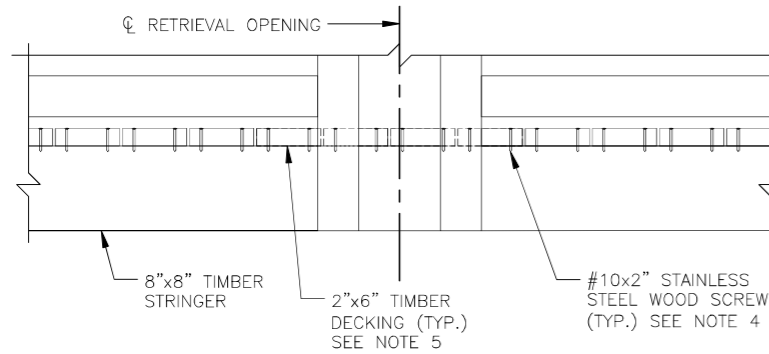
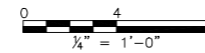
1. NEW TIMBER PILES TO BE DRIVEN TO MINIMUM PILE TIP ELEVATION. PILE INSTALLATION CRITERIA ESTABLISHED BY ENR FORMULA.
2. FOR HYDRAULIC DATA, SEE DWG. NO. EL-01.
3. (3) 3" DIA. WHITE REFLECTORS TO BE MOUNTED AT EACH PILE. CENTERLINE OF REFLECTOR TO BE AT EL. 3.5.
4. #10x2" STAINLESS STEEL WOOD SCREWS TO BE USED TO CONNECT TIMBER DECKING TO TIMBER STRINGERS. MINIMUM 2 SCREWS BETWEEN EACH TIMBER DECK PANEL AND EACH TIMBER STRINGER. MINIMUM GAP BETWEEN TIMBER WALKWAY BOARDS SHALL BE 1/4". MAXIMUM GAP BETWEEN TIMBER WALKWAY BOARDS SHALL BE 1/2".

- ↙ EL. 8.2
- ↙ EL. 5.4
- ▽▽ H.T.L. EL. 3.2
- ▽ C.J.L. EL. 3.0
- ▽ M.H.W. EL. 1.6
- ▽▽ M.L.W. EL. -1.2
- ▽ M.L.L.W. EL. -1.3

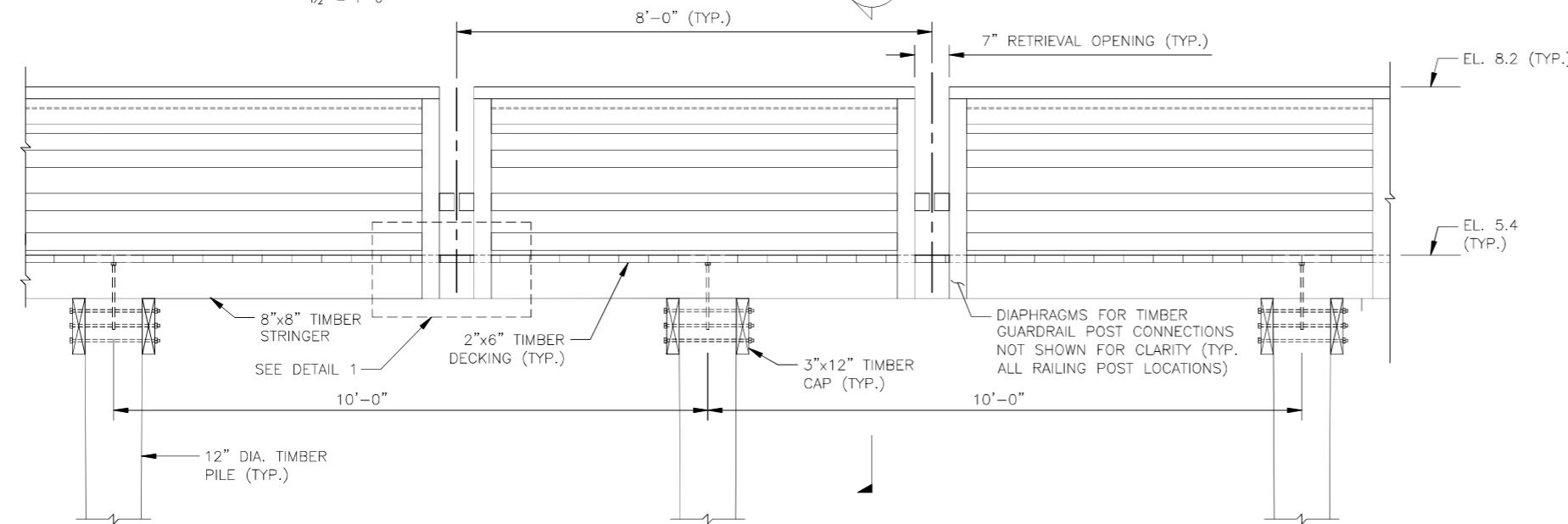
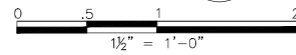
PILE TIP ELEVATION DATA			
SUBSTRUCTURE UNIT	PILE CUTOFF ELEVATION	ESTIMATED TIP ELEVATION	MINIMUM TIP ELEVATION
EAGLE LANDING FISHING PIER	+5.5 FT	-45 FT	-32 FT



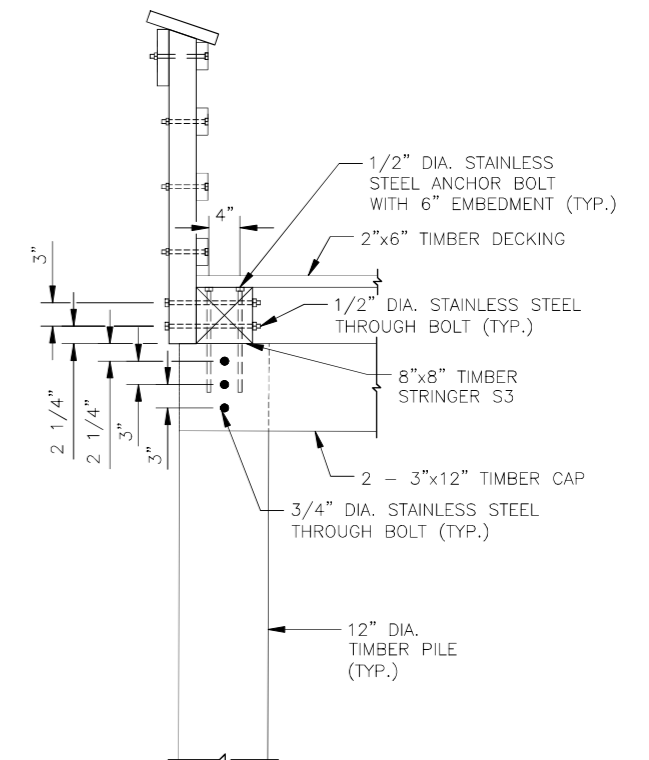
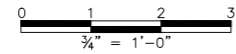
PROPOSED SOUTH ELEVATION



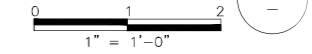
DETAIL 1



PROPOSED PARTIAL SOUTH ELEVATION



SECTION A



FILE NAME: 348R_EL-05-EAGLE LANDING ELEVATIONS.DWG
PLOT SCALE: AS NOTED
STANDARD PEN TABLE: YES

No.	Revisions	Date	By
0	ADDENDUM NO. 5 - NEW SHEET ADDED	10/13/23	HH

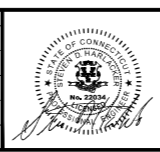
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Office of Chief Engineer
STRUCTURES

National Railroad Passenger Corporation
30th Street Station, Philadelphia, Pennsylvania 19104

Approved	Date



H&H | HARDESTY & HANOVER, LLC
ENGINEERING
1501 Broadway
New York, NY 10036

OLD SAYBROOK CONNECTICUT

**REPLACEMENT OF MB 106.89
OVER CONNECTICUT RIVER**

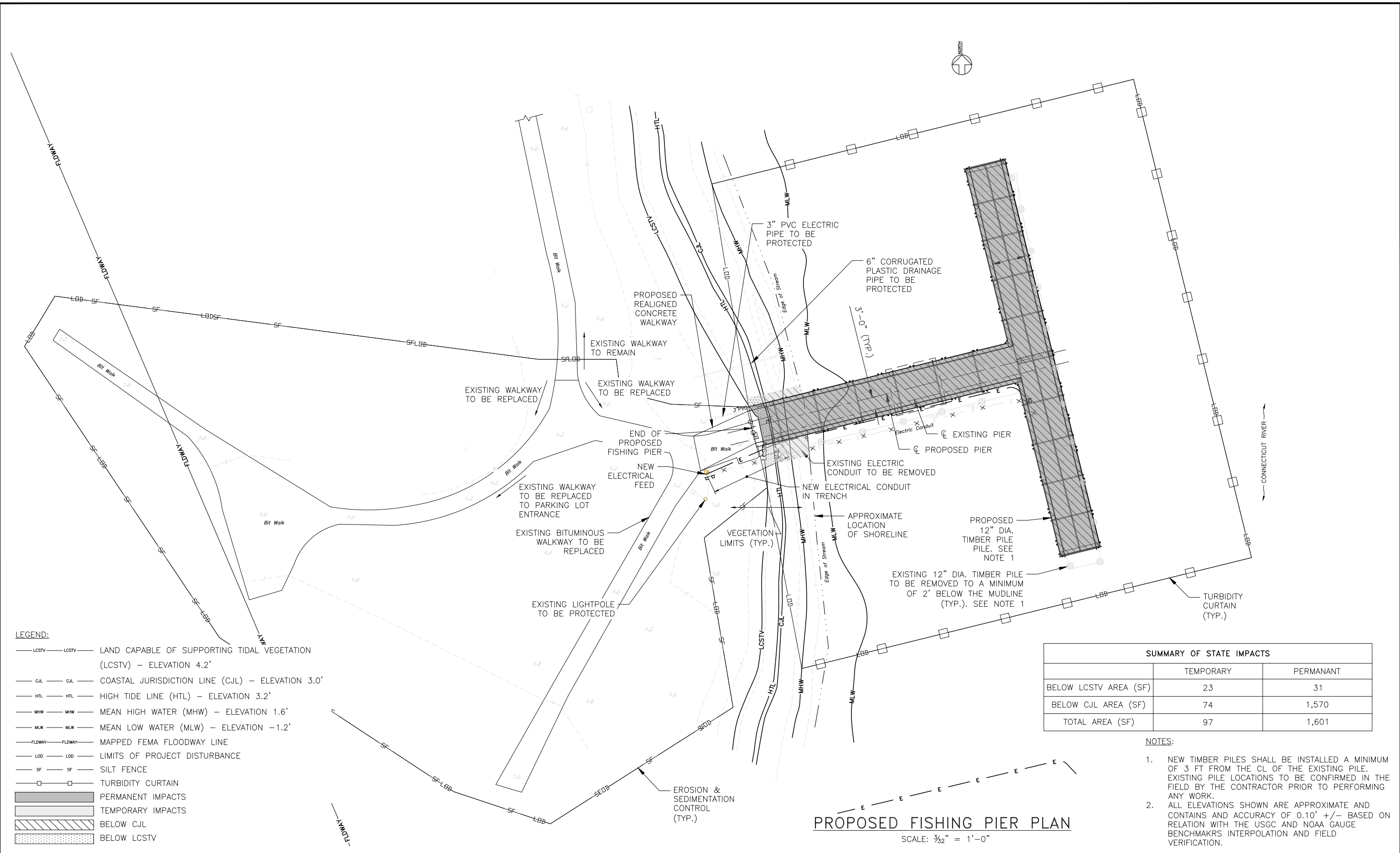
EAGLE LANDING FISHING PIER ELEVATIONS

Designed MY Drawn PD Checked MY Date 10/13/2023

Project Code: CT106.89

Sheet No. 1026.A5 OF 1026

Dwg. No. **EL-05**



LEGEND:

— LCSTV — LCSTV	LAND CAPABLE OF SUPPORTING TIDAL VEGETATION (LCSTV) — ELEVATION 4.2'
— C.J.L. — C.J.L.	COASTAL JURISDICTION LINE (CJL) — ELEVATION 3.0'
— HTL — HTL	HIGH TIDE LINE (HTL) — ELEVATION 3.2'
— MHW — MHW	MEAN HIGH WATER (MHW) — ELEVATION 1.6'
— MLW — MLW	MEAN LOW WATER (MLW) — ELEVATION -1.2'
— FLDWAY — FLDWAY	MAPPED FEMA FLOODWAY LINE
— LOD — LOD	LIMITS OF PROJECT DISTURBANCE
— SF — SF	SILT FENCE
□	TURBIDITY CURTAIN
■	PERMANENT IMPACTS
▨	TEMPORARY IMPACTS
▤	BELOW CJL
▥	BELOW LCSTV

SUMMARY OF STATE IMPACTS		
	TEMPORARY	PERMANANT
BELOW LCSTV AREA (SF)	23	31
BELOW C.J.L. AREA (SF)	74	1,570
TOTAL AREA (SF)	97	1,601

- NOTES:**
1. NEW TIMBER PILES SHALL BE INSTALLED A MINIMUM OF 3 FT FROM THE CL OF THE EXISTING PILE. EXISTING PILE LOCATIONS TO BE CONFIRMED IN THE FIELD BY THE CONTRACTOR PRIOR TO PERFORMING ANY WORK.
 2. ALL ELEVATIONS SHOWN ARE APPROXIMATE AND CONTAINS AND ACCURACY OF 0.10' +/- BASED ON RELATION WITH THE USGC AND NOAA GAUGE BENCHMARKS INTERPOLATION AND FIELD VERIFICATION.

PROPOSED FISHING PIER PLAN
SCALE: 3/32" = 1'-0"

FILE NAME: CDR IMPACT PLAN/EAGLE LANDING PROPOSED.DWG
DATE PLOTTED: 10/31/2023 10:58:15 AM
STANDARD PEN TABLE: YES

No.	Revisions	Date	By



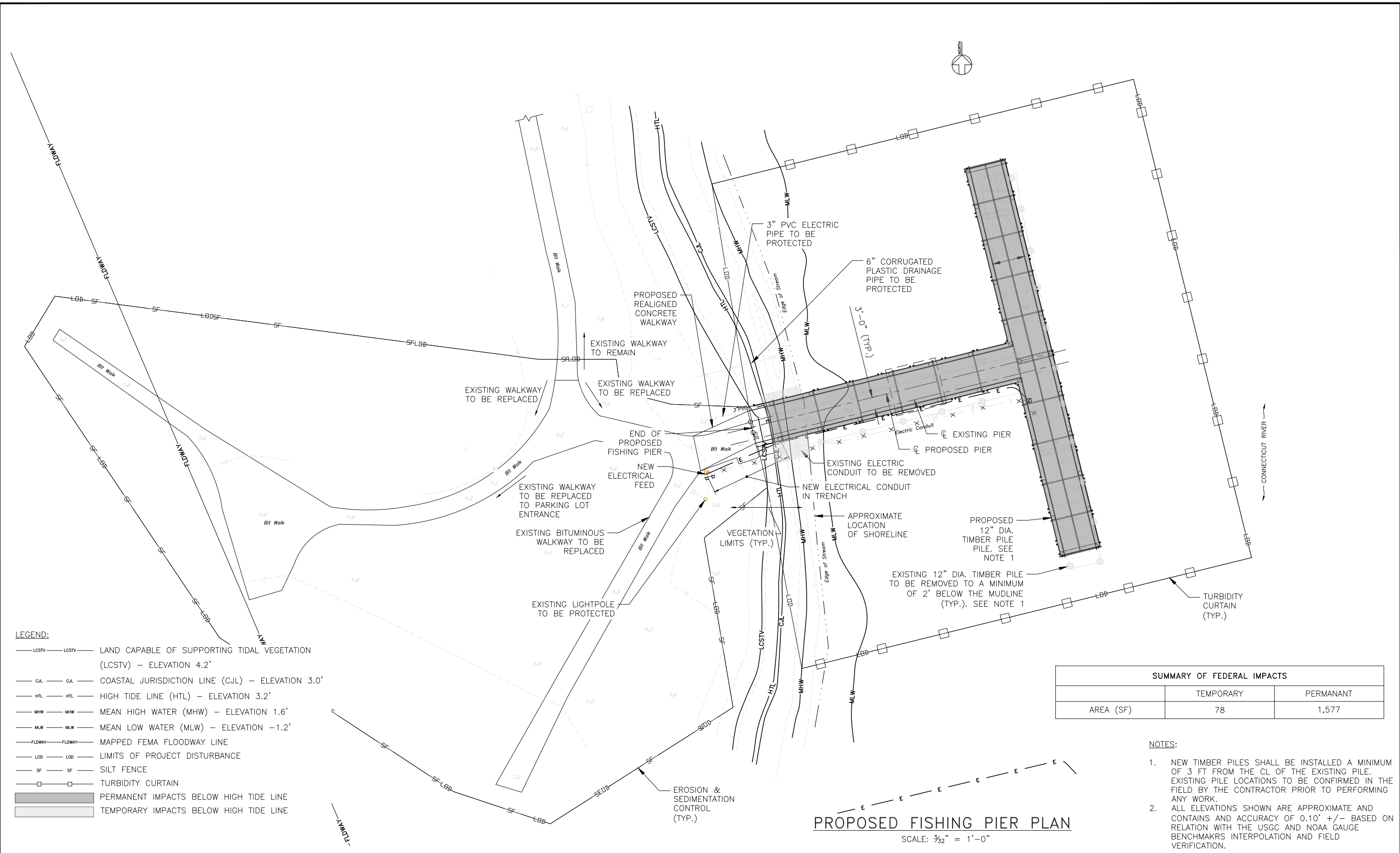
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STRUCTURES
National Railroad Passenger Corporation
30th Street Station, Philadelphia, Pennsylvania 19104

Approved	Date

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New York, NY 10036

OLD SAYBROOK CONNECTICUT
**REPLACEMENT OF MB 106.89
OVER CONNECTICUT RIVER**
EAGLE LANDING - STATE IMPACT PLAN
Designed MY Drawn AJA Checked BSH Date 10/31/2023

Project Code: CT106.89
Sheet No. 1 OF 5
Dwg. No. STE-1



LEGEND:

— LCSTV — LCSTV	LAND CAPABLE OF SUPPORTING TIDAL VEGETATION (LCSTV) — ELEVATION 4.2'
— CJL — CJL	COASTAL JURISDICTION LINE (CJL) — ELEVATION 3.0'
— HTL — HTL	HIGH TIDE LINE (HTL) — ELEVATION 3.2'
— MHW — MHW	MEAN HIGH WATER (MHW) — ELEVATION 1.6'
— MLW — MLW	MEAN LOW WATER (MLW) — ELEVATION -1.2'
— FLDWAY — FLDWAY	MAPPED FEMA FLOODWAY LINE
— LOD — LOD	LIMITS OF PROJECT DISTURBANCE
— SF — SF	SILT FENCE
— □ — □	TURBIDITY CURTAIN
■	PERMANENT IMPACTS BELOW HIGH TIDE LINE
▨	TEMPORARY IMPACTS BELOW HIGH TIDE LINE

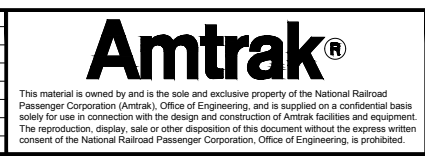
SUMMARY OF FEDERAL IMPACTS		
	TEMPORARY	PERMANANT
AREA (SF)	78	1,577

- NOTES:**
1. NEW TIMBER PILES SHALL BE INSTALLED A MINIMUM OF 3 FT FROM THE CL OF THE EXISTING PILE. EXISTING PILE LOCATIONS TO BE CONFIRMED IN THE FIELD BY THE CONTRACTOR PRIOR TO PERFORMING ANY WORK.
 2. ALL ELEVATIONS SHOWN ARE APPROXIMATE AND CONTAINS AND ACCURACY OF 0.10' +/- BASED ON RELATION WITH THE USGC AND NOAA GAUGE BENCHMARKS INTERPOLATION AND FIELD VERIFICATION.

PROPOSED FISHING PIER PLAN
SCALE: 3/32" = 1'-0"

FILE NAME: CDR IMPACT PLAN/EAGLE LANDING PROPOSED.DWG
DATE: 10/31/2023
DRAWN BY: AJA
CHECKED BY: BSH

No.	Revisions	Date	By



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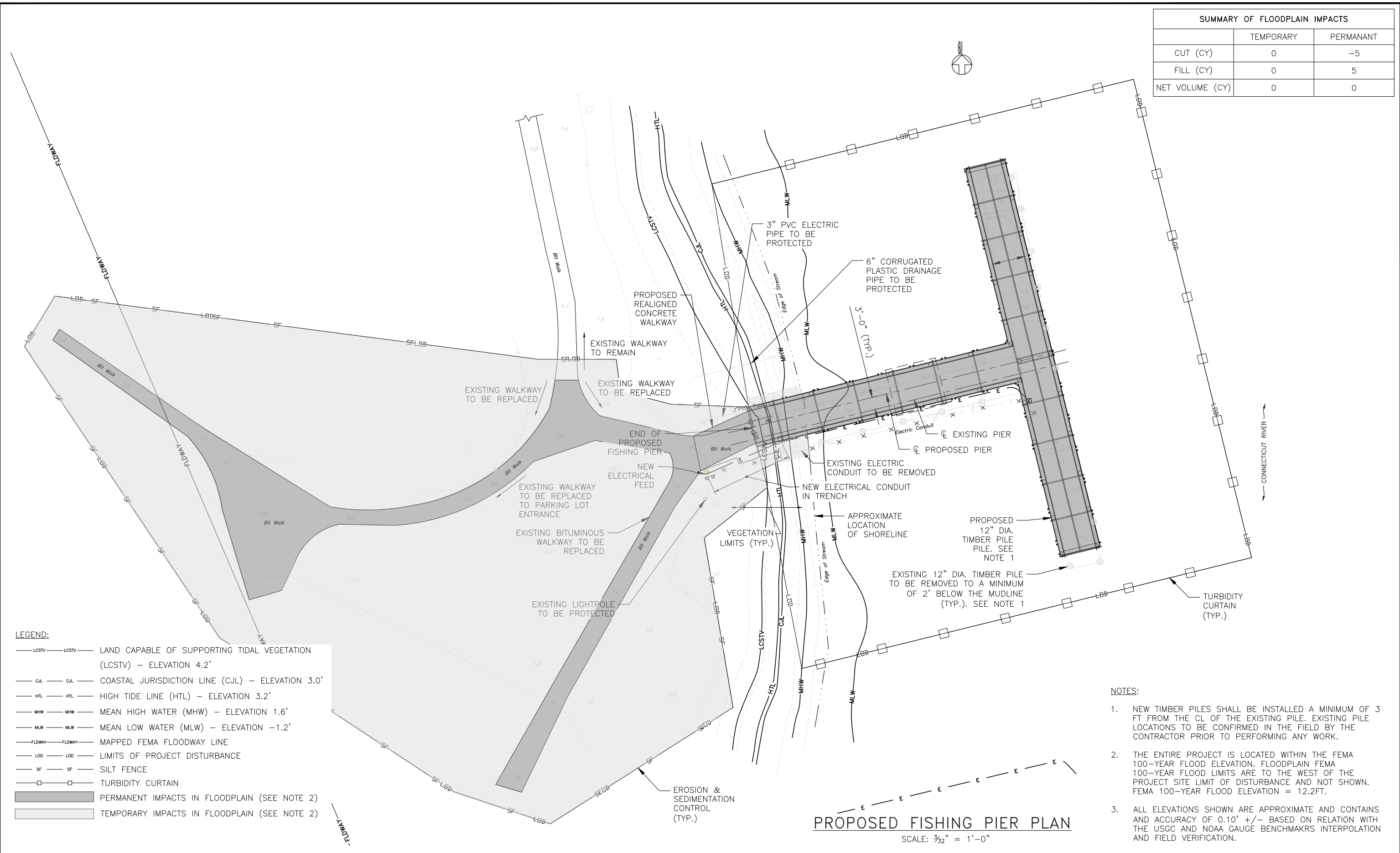
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New York, NY 10036

OLD SAYBROOK CONNECTICUT
**REPLACEMENT OF MB 106.89
OVER CONNECTICUT RIVER**
EAGLE LANDING - FEDERAL IMPACT PLAN
Designed MY Drawn AJA Checked BSH Date 10/31/2023

Project Code: CT106.89	Sheet No. 2 OF 5
Dwg. No. FED-1	

SUMMARY OF FLOODPLAIN IMPACTS		
	TEMPORARY	PERMANANT
CUT (CY)	0	-5
FILL (CY)	0	5
NET VOLUME (CY)	0	0



LEGEND:

— LCSTV — LCSTV	LAND CAPABLE OF SUPPORTING TIDAL VEGETATION (LCSTV) — ELEVATION 4.2'
— C.J.L. — C.J.L.	COASTAL JURISDICTION LINE (C.J.L.) — ELEVATION 3.0'
— HTL — HTL	HIGH TIDE LINE (HTL) — ELEVATION 3.2'
— MHW — MHW	MEAN HIGH WATER (MHW) — ELEVATION 1.6'
— MLW — MLW	MEAN LOW WATER (MLW) — ELEVATION -1.2'
— FLDWAY — FLDWAY	MAPPED FEMA FLOODWAY LINE
— LOD — LOD	LIMITS OF PROJECT DISTURBANCE
— SF — SF	SILT FENCE
□	TURBIDITY CURTAIN
■	PERMANENT IMPACTS IN FLOODPLAIN (SEE NOTE 2)
■	TEMPORARY IMPACTS IN FLOODPLAIN (SEE NOTE 2)

- NOTES:**
- NEW TIMBER PILES SHALL BE INSTALLED A MINIMUM OF 3 FT FROM THE CL OF THE EXISTING PILE. EXISTING PILE LOCATIONS TO BE CONFIRMED IN THE FIELD BY THE CONTRACTOR PRIOR TO PERFORMING ANY WORK.
 - THE ENTIRE PROJECT IS LOCATED WITHIN THE FEMA 100-YEAR FLOOD ELEVATION. FLOODPLAIN FEMA 100-YEAR FLOOD LIMITS ARE TO THE WEST OF THE PROJECT SITE LIMIT OF DISTURBANCE AND NOT SHOWN. FEMA 100-YEAR FLOOD ELEVATION = 12.2FT.
 - ALL ELEVATIONS SHOWN ARE APPROXIMATE AND CONTAINS AND ACCURACY OF 0.10' +/- BASED ON RELATION WITH THE USGC AND NOAA GAUGE BENCHMARKS INTERPOLATION AND FIELD VERIFICATION.

PROPOSED FISHING PIER PLAN
SCALE: 3/32" = 1'-0"

FILE NAME: CDR IMPACT PLAN/EAGLE LANDING PROPOSED.DWG
DATE: 10/31/2023
DRAWN BY: AJA
CHECKED BY: BSH
DESIGNED BY: MY

No.	Revisions	Date	By



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STRUCTURES
National Railroad Passenger Corporation
30th Street Station, Philadelphia, Pennsylvania 19104

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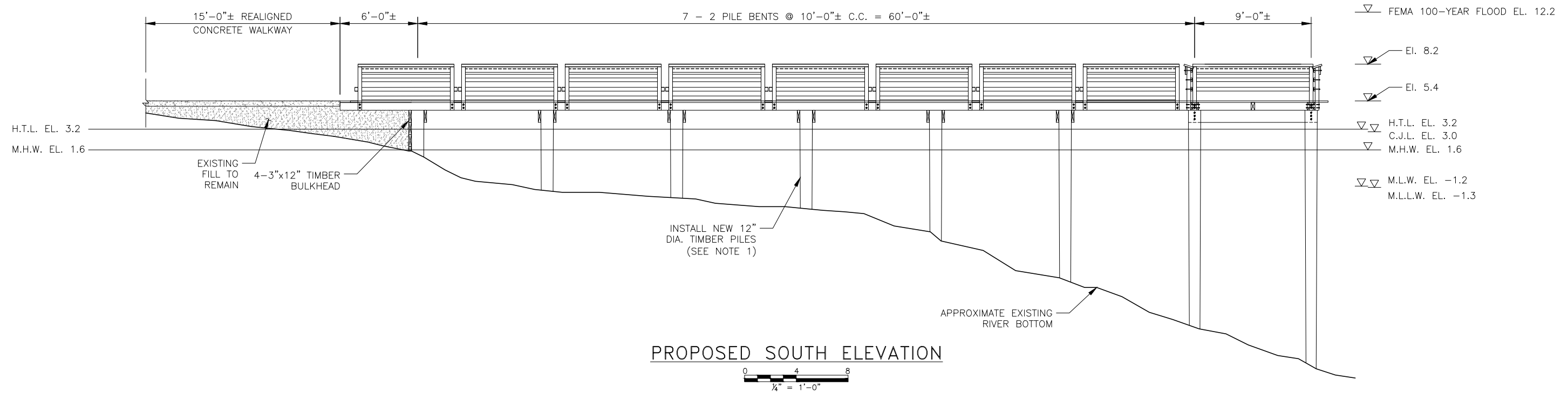
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OLD SAYBROOK CONNECTICUT
REPLACEMENT OF MB 106.89 OVER CONNECTICUT RIVER
EAGLE LANDING - FEMA IMPACT PLAN
Designed MY Drawn AJA Checked BSH Date 10/31/2023

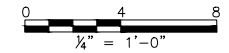
Project Code: CT106.89
Sheet No. 3 OF 5
Dwg. No. **FEMA-1**

NOTES:

1. ALL ELEVATIONS SHOWN ARE APPROXIMATE AND CONTAINS AN ACCURACY OF 0.10' +/- BASED ON RELATION WITH THE USGC AND NOAA GAUGE BENCHMARKS INTERPOLATION AND FIELD VERIFICATION.
2. FLOODPLAIN FEMA 100-YEAR FLOOD LIMITS ARE TO THE WEST OF THE PROJECT SITE LIMIT OF DISTURBANCE.



PROPOSED SOUTH ELEVATION



PILE TIP ELEVATION DATA			
SUBSTRUCTURE UNIT	PILE CUTOFF ELEVATION	ESTIMATED TIP ELEVATION	MINIMUM TIP ELEVATION
EAGLE LANDING FISHING PIER	+5.5 FT	-45 FT	-32 FT

FILE NAME: 3486_C05_IMPACT ELEVATION EAGLE LANDING.DWG
DATE PLOTTED: 7/14/2023 10:58:10 AM
STANDARD PEN TABLES

No.	Revisions	Date	By
0	ADDENDUM NO. 5 - NEW SHEET ADDED	10/13/23	HH

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Office of Chief Engineer
STRUCTURES
National Railroad Passenger Corporation
30th Street Station, Philadelphia, Pennsylvania 19104

Approved	Date

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1501 Broadway
New York, NY 10036

OLD SAYBROOK CONNECTICUT
REPLACEMENT OF MB 106.89
OVER CONNECTICUT RIVER
EAGLE LANDING - FEMA IMPACT ELEVATION
Designed MY Drawn AJA Checked BSH Date 7/14/2023

Project Code:	CT106.89
Sheet No.	3 OF 5
Drawn No.	FEMA-2

SUGGESTED CONSTRUCTION SEQUENCE:

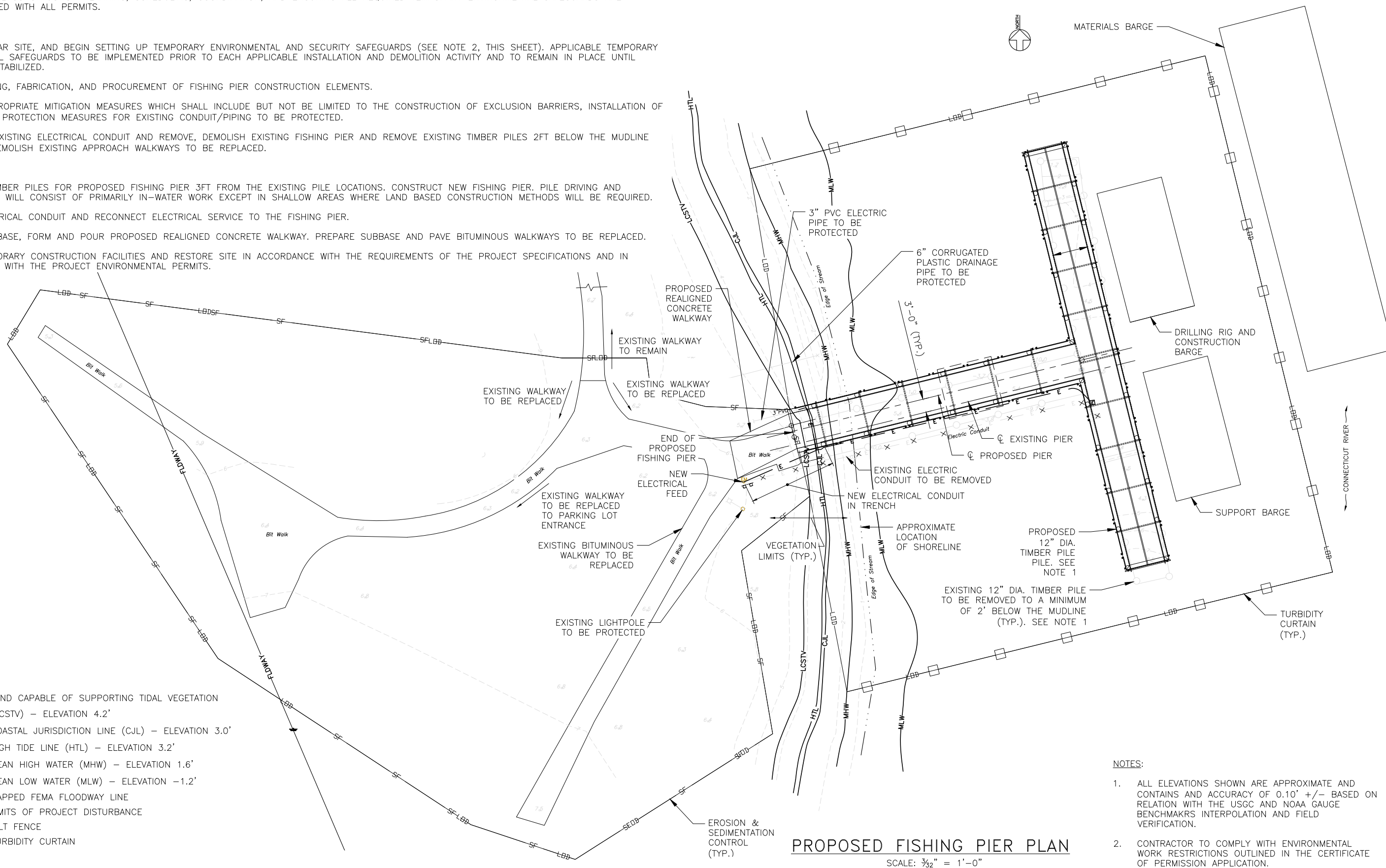
THIS SUGGESTED CONSTRUCTION SEQUENCE PROVIDES A SUMMARY OUTLINE FOR A POTENTIAL SEQUENCE OF CONSTRUCTION ACTIVITIES. THE CONTRACTOR IS RESPONSIBLE FOR ACTUAL CONSTRUCTION PLANNING, SCHEDULING, COORDINATION, AND ENSURING ALL REQUIRED TEMPORARY ENVIRONMENTAL SAFEGUARDS ARE INSTALLED AND MAINTAINED WITH ALL PERMITS.

PHASE 1:

- 1A1. MOBILIZE, CLEAR SITE, AND BEGIN SETTING UP TEMPORARY ENVIRONMENTAL AND SECURITY SAFEGUARDS (SEE NOTE 2, THIS SHEET). APPLICABLE TEMPORARY ENVIRONMENTAL SAFEGUARDS TO BE IMPLEMENTED PRIOR TO EACH APPLICABLE INSTALLATION AND DEMOLITION ACTIVITY AND TO REMAIN IN PLACE UNTIL LOCATION IS STABILIZED.
- 1A2. BEGIN PLANNING, FABRICATION, AND PROCUREMENT OF FISHING PIER CONSTRUCTION ELEMENTS.
- 1A3. PERFORM APPROPRIATE MITIGATION MEASURES WHICH SHALL INCLUDE BUT NOT BE LIMITED TO THE CONSTRUCTION OF EXCLUSION BARRIERS, INSTALLATION OF FENCING, AND PROTECTION MEASURES FOR EXISTING CONDUIT/PIPING TO BE PROTECTED.
- 1A4. DISCONNECT EXISTING ELECTRICAL CONDUIT AND REMOVE, DEMOLISH EXISTING FISHING PIER AND REMOVE EXISTING TIMBER PILES 2FT BELOW THE MUDLINE (MINIMUM.), DEMOLISH EXISTING APPROACH WALKWAYS TO BE REPLACED.

PHASE 2:

- 2A1. DRIVE NEW TIMBER PILES FOR PROPOSED FISHING PIER 3FT FROM THE EXISTING PILE LOCATIONS. CONSTRUCT NEW FISHING PIER. PILE DRIVING AND CONSTRUCTION WILL CONSIST OF PRIMARILY IN-WATER WORK EXCEPT IN SHALLOW AREAS WHERE LAND BASED CONSTRUCTION METHODS WILL BE REQUIRED.
- 2A2. INSTALL ELECTRICAL CONDUIT AND RECONNECT ELECTRICAL SERVICE TO THE FISHING PIER.
- 2A3. PREPARE SUBBASE, FORM AND POUR PROPOSED REALIGNED CONCRETE WALKWAY. PREPARE SUBBASE AND PAVE BITUMINOUS WALKWAYS TO BE REPLACED.
- 2A4. REMOVE TEMPORARY CONSTRUCTION FACILITIES AND RESTORE SITE IN ACCORDANCE WITH THE REQUIREMENTS OF THE PROJECT SPECIFICATIONS AND IN CONFORMANCE WITH THE PROJECT ENVIRONMENTAL PERMITS.



LEGEND:

— LCSTV — LCSTV	LAND CAPABLE OF SUPPORTING TIDAL VEGETATION (LCSTV) — ELEVATION 4.2'
— CJL — CJL	COASTAL JURISDICTION LINE (CJL) — ELEVATION 3.0'
— HTL — HTL	HIGH TIDE LINE (HTL) — ELEVATION 3.2'
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— FLDWAY — FLDWAY	MAPPED FEMA FLOODWAY LINE
— LOD — LOD	LIMITS OF PROJECT DISTURBANCE
— SF — SF	SILT FENCE
□ — □	TURBIDITY CURTAIN

- NOTES:**
- ALL ELEVATIONS SHOWN ARE APPROXIMATE AND CONTAINS AND ACCURACY OF 0.10' +/- BASED ON RELATION WITH THE USGC AND NOAA GAUGE BENCHMARKS INTERPOLATION AND FIELD VERIFICATION.
 - CONTRACTOR TO COMPLY WITH ENVIRONMENTAL WORK RESTRICTIONS OUTLINED IN THE CERTIFICATE OF PERMISSION APPLICATION.

PROPOSED FISHING PIER PLAN
SCALE: 3/32" = 1'-0"

FILE NAME: CDR IMPACT PLAN/EAGLE LANDING PROPOSED.DWG
DATE: 10/31/2023
DRAWN BY: AJA
CHECKED BY: BSH

No.	Revisions	Date	By



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STRUCTURES**
National Railroad Passenger Corporation
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New York, NY 10036

OLD SAYBROOK	CONNECTICUT	Project Code: CT106.89
REPLACEMENT OF MB 106.89 OVER CONNECTICUT RIVER		
EAGLE LANDING - STAGING PLAN		
Designed MY	Drawn AJA	Checked BSH
Date 10/31/2023		
Dwg. No. STG-1		